

DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

PURPOSE

The purpose of the Development Standards and Design Guidelines is to guide future development in the Midtown Area to be consistent with the vision and goals for the area as detailed in the Specific Plan. These standards describe and illustrate building and landscape designs that are appropriate for the Midtown Area. They establish the criteria used by the City in reviewing proposed development. They are intended to encourage high quality design and development, creativity and innovation in Midtown Milpitas.

The guidelines included herein indicate the minimum requirements, and in some cases, developers may be required to provide more than the minimum in order to meet the stated intent of the Specific Plan.

USE OF STANDARDS & GUIDELINES

This section contains both mandatory regulations in the form of development standards and interpretive design guidelines, which allow some level of discretion in achieving the established objectives.

The words “shall” and “will” indicate a mandatory requirement. The word “should” means that an action is required unless a determination is made that the intent of the guideline is satisfied by other means. Words such as “encouraged” or “may” are advisory and are provided as guidelines for development. In general, the word “shall” is used in the Development Standards. The Design Guidelines include the word “should” indicating a mandatory guideline.

The Specific Plan introduces two new General Plan and Zoning designations—these include the multi-family very high density (R4) and the mixed-use district (MXD). In addition, the Specific Plan also proposes two overlay zones over the underlying zoning and land use designations. These overlay zones include the TOD overlay zone and the Gateway Office overlay zone. Combining these overlay districts with the two proposed General Plan and zoning designations results in five new categories, listed below.

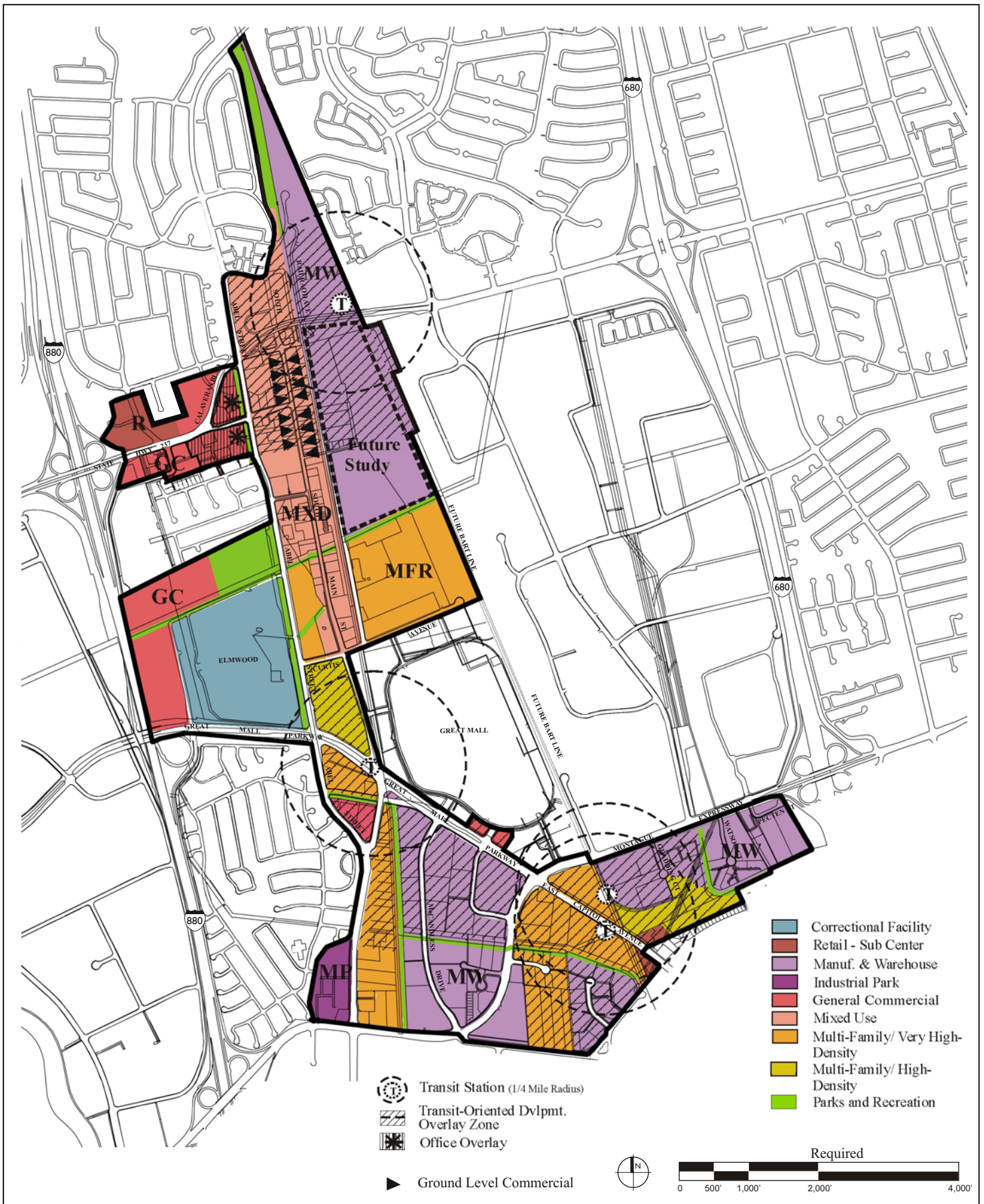


Figure 8.0: Midtown Land Use Plan

EDAW

March 2002

These designations include:

General Plan	Zoning
Multifamily Very High Density	R4
Mixed-Use District	MXD
Mixed-Use with a TOD Overlay Zone	MXD-TOD
Multifamily Very High Density with a TOD Overlay Zone	R4-TOD
Gateway Office Overlay Zone	C2-OO

The Specific Plan includes the Gateway Office overlay zone designation on two specific sites with an underlying commercial land use designation with the purpose of attracting Class A office buildings.

There are existing land use and zoning designations in the Midtown Area (such as C1, C2, M1, M2, MP etc.). Since no zoning ordinance changes are proposed to the existing land use and zoning designations in the Midtown Area, standards and regulations for those areas are not included in this section. For regulations and policies for existing land use and zoning designations, please refer to the City's General Plan and Zoning Ordinance. However, the design guidelines included herein are applicable to both the existing and proposed

land use and zoning designations.

FORMAT

A development standards matrix summarizing the standards is included at the end of this section.

The design guidelines are discussed under the following headings:

A. Site Planning

1. Street Pattern
2. Site Configuration and Design
3. Parking Areas
4. Treatment of Garage Frontage in Residential and Mixed-Use Buildings
5. Service Areas in Non-Residential Projects

B. Building Design

1. Building Orientation
2. Building Massing and Articulation
3. Fenestrations
4. Building Materials
5. Building Colors
6. Roof Design

C. Open Space and Landscaping

1. Landscaping
2. Accessway and Drive Aisle Landscaping
3. General Planting Guidelines
4. Signage
5. Lighting

D. Design Guidelines by Building Type

1. Mixed-Use Buildings
2. Multifamily Residential
3. Large Floor Plate or Big Box Retail
4. Office Buildings
5. Class A Office Building
6. Civic, Public and Quasi-Public Buildings
7. Light Industrial/Industrial Park
8. Parking Structures

E. Design Guidelines for Specific Projects

1. Town Square
2. Transit Stations
3. South Main Street Residential Development

RELATIONSHIP TO OTHER DOCUMENTS

The City is amending its General Plan and Zoning Regulations concurrently with the adoption of the Specific Plan, in order to ensure consistency with the plan, including the Development Standards and Design Guidelines. Should a conflict between these Guidelines and Standards and the Milpitas Zoning Code or Sign Ordinance arise, the standards contained within this section shall govern. All other sections of the Milpitas Municipal Code shall prevail over the Design Guidelines and Standards. This section, like the entire Specific Plan, may be modified only with the approval of the City Council, through a formal Specific Plan modification process.

REVIEW PROCESS

All projects proposed within the Midtown Area are subject to a Site and Architectural Review, (S-Zone Review), in accordance with Chapter 42 of the City's Zoning Ordinance. In addition to the usual S-Zone process of reviewing projects for conform-

ance with the City’s General Plan and Zoning Ordinance, projects shall have to demonstrate compliance with the Specific Plan—including the Development Standards and Design Guidelines. No S-Zone approval shall be issued by the City without the decision making body making the following finding:

“The proposed project conforms to the intent and the specific requirements of the Midtown Specific Plan, including the Development Standards and Design Guidelines.”

Exceptions to the standards may be approved by the Planning Commission upon review of a use permit, in accordance with the requirements of Chapter 57 of the Zoning Code. This process may not be used to vary from the density requirements, allowable uses or public and private park land requirements contained within the standards or the Zoning Code. In addition to the required finding under Chapter 57, the Planning Commission must be able to make the following two additional findings:

“The deviation from the Midtown Specific Plan Standard meets the design intent identified

within the Specific Plan and does not detract from the overall architectural, landscaping and site planning integrity of the proposed development.”

“The deviation from the Midtown Specific Plan Standard allows for a public benefit not otherwise obtainable through the strict application of the Design Standard.”

The City is consciously choosing to apply the use permit process rather than the variance process when allowing exceptions to the Design Standards in order to allow for the maximum flexibility in meeting the intent of the Specific Plan.

1.0 Density

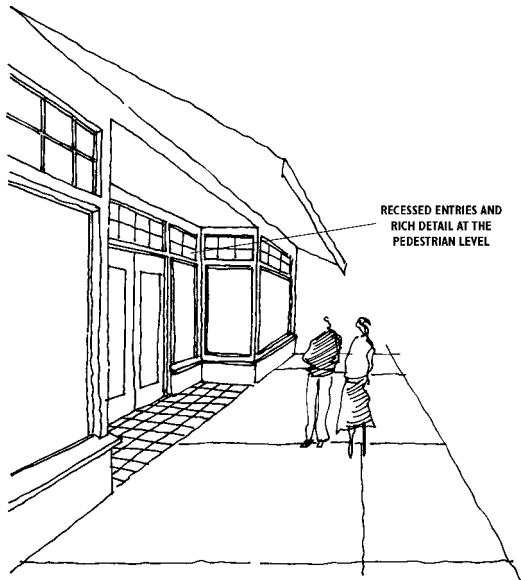
Density refers to the amount of development that is permitted in the various zoning districts. For residential uses, the density is measured as dwelling units per gross acre. For non-residential uses, the density is defined by the FAR. An FAR is the ratio of the total building area to the gross site area. For mixed-use buildings, the density refers to the residential component of the project, while a separate FAR is included for the commercial component of the project. In the mixed-use district, density and FAR are calculated independently.

The densities for the various zoning designations are as follows:

- | | |
|--|---|
| <p>a. R4 (Multifamily Very High Density)</p> <ul style="list-style-type: none"> • 31 dwelling units per gross acre (minimum) • 40 dwelling units per gross acre (maximum) • For parcels less than 20,000 square feet, minimum number of residential units may be reduced with approval of the Planning Commission. | <ul style="list-style-type: none"> • FAR: 0.75 |
| <p>b. MXD</p> <ul style="list-style-type: none"> • 21 dwelling units per gross acre (minimum) • 30 dwelling units per gross acre (maximum) • For parcels less than 20,000 square feet, minimum number of residential units may be reduced with approval of the Planning Commission. | <p>c. MXD–TOD (Mixed-Use District with a TOD Overlay Zone)</p> <ul style="list-style-type: none"> • 31 dwelling units per gross acre (minimum) • 40 dwelling units per gross acre (maximum) • FAR 1.0 |
| | <p>d. R4–TOD (Multifamily Very High Density with a TOD)</p> <ul style="list-style-type: none"> • 41 dwelling units per gross acre (minimum) • 60 dwelling units per gross acre (maximum) FAR: 0.50 |
| | <p>e. C2-OO (Gateway Office Overlay Zone)</p> <ul style="list-style-type: none"> • FAR: 1.5 for Class A office buildings |

2.0 Maximum Building Height

- a. *R4 (Multifamily Very High Density)*
 - 4 stories and 60 feet, including special architectural elements such as towers and spires.
- b. *MXD (Mixed-Use District)*
 - 3 stories and 45 feet, including special architectural features such as towers or corner elements up to 55 feet.
- c. *MXD–TOD (Mixed-Use District with a TOD Overlay Zone)*
 - 4 stories and 60 feet, including special architectural elements such as towers and spires.
- d. *R4–TOD (Multifamily Very High Density with a TOD)*
 - 5 stories and 75 feet, including special architectural elements such as towers and spires.
- e. *C2-00 (Gateway Office Overlay Zone)*
 - 6 stories and 85 feet. A conditional use permit may be approved by the Planning Commission for buildings that exceed this standard up to 8 stories and not more than 115 feet in height for exceptional architecture and aesthetic merit.



A recessed door entrance up to 9 feet wide and 4 feet from the back of sidewalk.



Bay Meadows I Townhomes, San Mateo. This illustrates stairs and private porches built in the frontyard setback.

3.0 Building Setbacks

Building setbacks indicate the distance between the outer edge of the building facade and the property line, or edge of sidewalk or curbs. Also included within the category of setbacks is the concept of build-to lines. A build-to line is generally used in urban areas, (such as Main Street) to define locations where buildings must be built within a certain distance of the public right-of-way. A build-to line ensures that the various buildings along the street create a defined building edge, which helps create a comfortable pedestrian setting along the sidewalk. Where build-to lines are required, a minimum of 60% of the street facing building facade should be located on the build-to line. All public sidewalks shall have a minimum width of 10 feet from the edge of the curb, except for the Core Main Street area (as shown in Figure 3.1), where the sidewalks shall have a minimum width of 15 feet.

a. R4 (Multifamily Very High Density)

1. Front Setbacks:

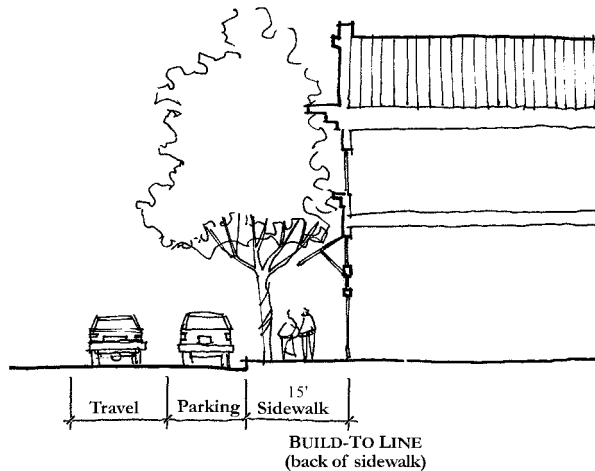
A minimum of 8 feet and a maximum of 15 feet from back of sidewalk. The sidewalk shall either be based on the existing sidewalk or an assumed 10-foot wide sidewalk, whichever is wider. Within the 10-foot sidewalk shall be street trees.

- Porches and stairs may project up to 6 feet into the minimum setback provided they are incorporated into an integrated landscape concept where the majority of the setback area is reserved for landscaping.
- Parking is prohibited in the front setback.

- Setbacks shall be attractively landscaped with low hedges, flowering shrubs and specimen trees planted in the same configuration as the street trees to create a pedestrian colonnade along the sidewalk.

2. Side and Rear Yard Setbacks:

- 10 foot minimum.
- Setback areas shall be landscaped but may also be occupied by residential accessory buildings, or drive aisles.
- Balconies, bay windows, and awnings may project up to 6 feet into the setbacks.
- Street side yards shall be treated the same as front setbacks.



A build-to line (0 feet setback) at the back of sidewalk.



This mixed-use development illustrates the “build-to line” concept.

b. MXD (Mixed-Use District)

1. Front Build-To Line/Setbacks:

For buildings along the Ground Level Commercial portion of Main Street (as identified on Figure 3.1): Front setback shall be a build-to line 15 feet from the edge of the curb. Within the 15 feet sidewalk shall be street trees.

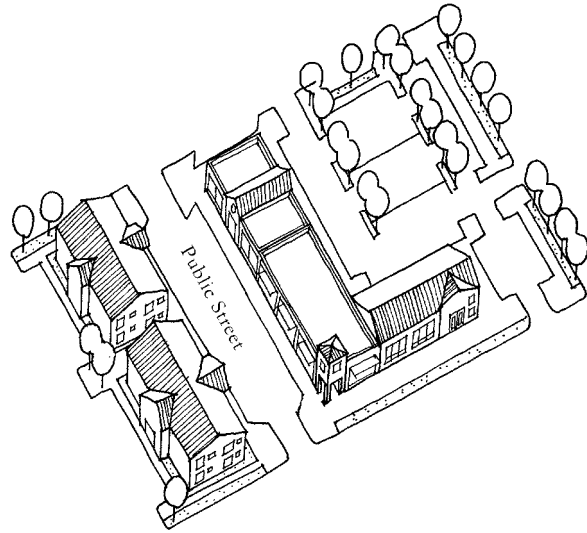
All other areas: A minimum setback of 8 feet and a maximum of 15 feet from the back of sidewalks. The sidewalk shall either be based on the existing sidewalk or an assumed 10-foot wide sidewalk, whichever is wider. Within the 10-foot sidewalk shall be street trees.

- A building’s first floor may be recessed from the front setback for the purposes of an arcade, or a small gathering/dining or special entry area. The arcade should have a minimum height of 8 feet, a minimum width of 8 feet.
- The frontyard setback may be reduced for mixed-use developments where it can be demonstrated that such a change is compatible with adjacent properties and would enhance the streetscape environment.

- Recessed areas may have maximum depth of 10 feet, and may not exceed 40% of the building’s street facing elevation.
- An entry door area up to 9 feet wide may be recessed up to 4 feet from the back of the sidewalk.
- Balconies, bay windows, porches, stoops and awnings may project into the setbacks provided at least 60% of the required setback area is landscaping.
- Trellises, canopies and fabric awnings may project up to 5 feet into front setbacks and public rights-of-way, provided they are not less than 8 feet above sidewalk.
- All buildings must face onto the street they front upon.
- All required front setback areas shall be landscaped or paved to allow for outdoor seating or street furniture.
- Setbacks shall be well landscaped with low hedges, flowering shrubs and trees that create diversity and interest along the streets.

2. Side and Rear Yard Setbacks:

- In the ground level commercial area, no interior side yard setback required.



Off-street parking located and accessed from the rear of the buildings.

- 10 feet from the side and rear yard property lines.
- Setback areas shall be landscaped but may also be occupied by residential accessory buildings or drive aisles.
- To mitigate the effects of adjacent service commercial or light industrial uses, increased setbacks and other measures, such as a solid, 6 foot fence or masonry wall, shall be considered by the Planning Commission during the review process.
- Streetside setback shall be treated same as front setback.

c. C2-00 (Gateway Office Overlay Zone)

1. Front Setback:

- A minimum setback of 0 feet and a maximum of 10 feet from back of sidewalk.
- The main entrance shall face the street.
- The building shall be parallel to the street.

4.0 Off-Street Parking

a. R4 (Multifamily Very High Density)

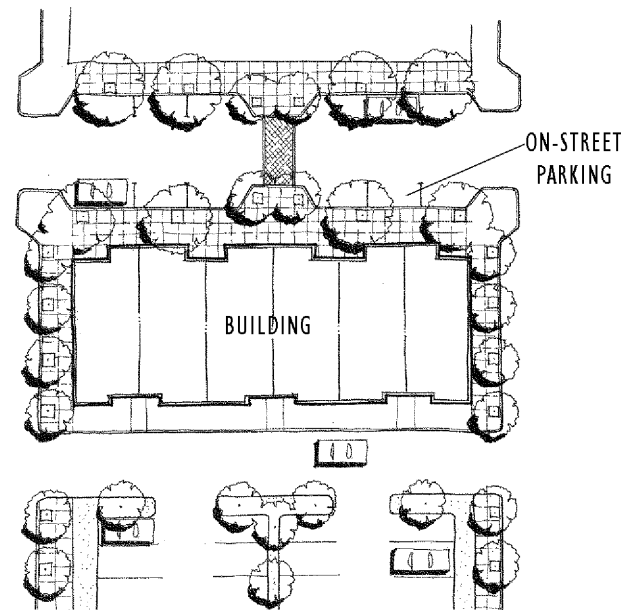
Multiple family dwellings shall include the following ratios of parking:

Unit Size	Stalls per Unit
Studio	1.0 covered
1 Bedroom	1.5 covered
2 Bedroom	2.0 covered
3 Bedroom	2.0 covered
Guest Parking	15% of required stalls
Bicycle Parking	5% of total stalls

- Parking is prohibited in the front setback and should be at the rear of the buildings.
- Shared driveways shall be considered wherever possible to minimize curbcuts.
- Carports shall be integrated with the character and materials of the building architecture and landscaping.
- A single carport shall not be more than 8 stalls wide.
- Carports shall be separated from one another with a 4-foot-wide (minimum interior dimension) landscaped island, planted with a tree.



The on-street parking on the buildings street frontage can be added toward the development's total parking requirement.



On-street parking in the front of the buildings, with surface parking lots accessed from the rear.

- Tandem parking in garages is allowed.

b. MXD (Mixed-Use District)

The parking ratios for buildings in the mixed-use district are as follows:

Unit Size	Stalls per Unit
Single Family Dwellings	2.0 covered
Multifamily Dwellings	
<i>Studio</i>	1.0 covered
<i>1 Bedroom</i>	1.5 covered
<i>2 Bedroom</i>	2.0 covered
<i>3 Bedroom</i>	2.0 covered
<i>Guest Parking</i>	15% of required stalls
<i>Bicycle Parking</i>	5% of total stalls
Retail	4 spaces per 1,000 gross sf.
Restaurant	Refer to zoning code
Public/Quasi-Public	Refer to zoning code
Office Buildings * <1,000 sf.	5 spaces per 1,000 gross sf.
Office Buildings * >1,000 sf.	3.3 spaces per 1,000 gross sf.
Other Uses	Refer to Zoning Code

sf = square feet

- On-street parking adjacent to the building's street frontage may be counted toward a development's overall parking requirement.
- Parking is prohibited in the front setback and should be at the rear of the buildings.
- Shared driveways shall be considered wherever possible to minimize curbcuts.

c. TOD (Mixed-Use District with a TOD Overlay Zone)

- Parking requirements, as listed in the MXD (Mixed-Use District), will be reduced by 20% of the total required parking.
- The northern TOD, may only apply this reduction once a decision on the location and the viability of the northern BART station has been made by the City Council.



Two multifamily residential developments which include public parks for adjacent residents.

5.0 Parks & Open Space

The Specific Plan envisions three kinds of open spaces within the Midtown Area. Public parks are community open spaces that are publicly-accessible and meant for use (e.g. Town Square, Transit Green, pedestrian and bicycle trails). Common open spaces are those that are incorporated into a housing development. These spaces could include private park areas with uses such as swimming pools, tot-lots, club houses, exercise rooms, large lawn areas for playing and tennis courts. Common open space also includes landscaped areas that create the environment within the development. Private open space includes patios and balconies.

a. Park Requirements

- Developments in all areas of Midtown shall provide public and private park space at a ratio of 3.5 acres per 1,000 population.
- Up to 1.5 of the 3.5 acres of park space required may be provided as private park space, the remainder shall be public park space.
- All public parks shall be publicly-accessible.
- Parks shall be evenly dispersed through the development with a goal of providing a park or open space in close proximity to the residential units.
- Private park space should be configured to be usable for recreational purposes by residents of the housing development.

b. Open Space

- For developments in the R4 District at least 25% of the site shall be open space (exclusive of parking lots and roadways).
- For developments in the R4, a minimum of 200 square feet of usable open space per unit (may include an on-site private park or a public park).
- In the MXD, MXD-TOD, and R4-TOD, open space requirements are to the approval of the Planning Commission.

6.0 Utilities

a. Utilities

- Utilities shall be placed in underground or subsurface conduits.
- All mechanical equipment, ground transformers, and meters shall be located to minimize visual impacts, particularly from public views, and shall be adequately screened with planting, berms or with an enclosure.
- Roof-mounted mechanical equipment shall be concealed from ground-level views through a roof design that is architecturally integrated with the building, such as equipment wells and parapets.
- Public utility distribution meters, vaults, and similar installations shall be consolidated in a single area whenever possible and located away from highly visible areas such as street corners and public open spaces. Their locations shall be coordinated with lighting and street trees to minimize impacts to street landscaping.
- Equipment and its enclosures shall be adequately screened with landscaping and blend with surroundings.

b. Backflow Preventors

- Backflow preventors shall be located within landscaped setback areas and painted black or dark green to minimize visual appearance.
- Where no landscaped setback areas exist, backflow preventors shall be incorporated into the front of buildings to minimize visual obtrusiveness into sidewalks and pedestrian promenades.
- Exterior mounted utility equipment should be painted to blend with its surroundings.

c. Trash/Recycling Enclosures

- Larger refuse and recycling containers used by the multifamily and mixed-use buildings shall not be visible from a public or private street. Such containers shall be stored either within the parking facility of the building or within a vehicular accessway with appropriate screening.
- Trash receptacle pads shall be integrated within the design of the residential lanes.
- All enclosure walls shall incorporate the building materials and colors to match the architecture of the building, additionally, they shall include appropriate landscaping for screening.

d Telecommunication Facilities

- All antennas for cellular and telecommunication uses shall be building facade or roof-mounted and screened appropriately. The smallest available antennas shall be used in the Midtown Area.
- On Main Street, the cellular antennas shall be “stealth” facilities.

7.0 Minimum Spatial Requirements For Quasi-Public And Other Specified Uses

In the Mixed Use District, there is a concentration of quasi-public uses and businesses related to vehicles and industrial services. These types of uses have limited daytime activity and do not contribute to an active street environment. Therefore, in order to promote a lively mix of uses in the Mixed Use District, the following spatial distance requirements shall apply to new uses.

a. Quasi-Public Uses

The following quasi-public uses may be permitted in the Mixed Use District with approval of a Conditional Use Permit and provided they are not located within 1,000 feet of another quasi-public use listed below:

1. Places of assembly, including but not limited to auditoriums, banquet halls, fraternal or union hall, churches and religious institutions.
2. Hospitals or sanitariums.
3. Private elementary, middle or high schools.

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4. Vocational schools, if not found objectionable due to noise, odor, vibration, or other similar health, safety or welfare basis.

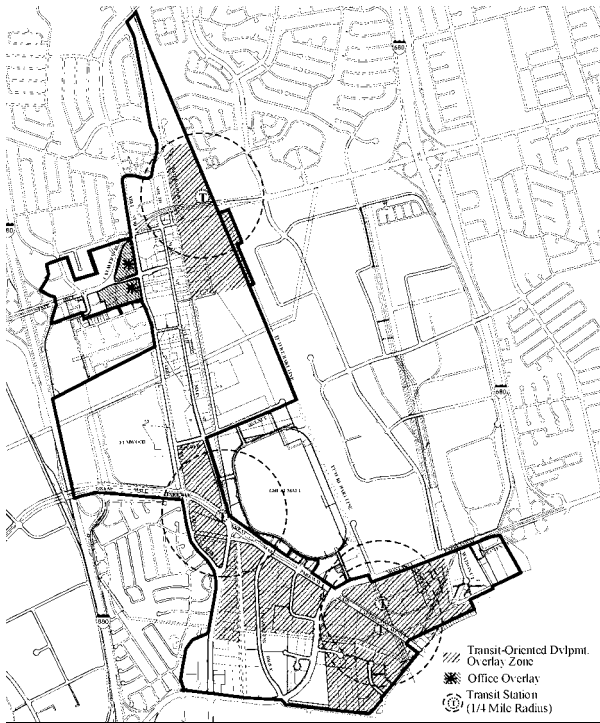
b. Other Specified Uses

The following uses may be permitted in the Mixed Use District with approval of a Conditional Use Permit and provided they are not located within 1,000 feet of the same type of use or any use listed below:

1. Vehicle service uses, including but not limited to: gasoline service stations, car washes, tire shops, towing without vehicle storage, and vehicle repair shops of all kinds, glass, upholstery, etc. Entrances to the service bays shall not be open to the street but shall be designed to face the rear or interior side property line.
2. Cabinet or carpenter shops if conducted in a completely enclosed building.
3. Janitorial services and window cleaning services.
4. Local transportation service facilities (e.g. taxi, parcel service, ambulance, armored car, van storage and auto rental).
5. Pawnshops.

6. Plumbing or sheet metal shops.
7. Sign shops, if conducted wholly within completely enclosed buildings.

The provisions relating to the location of quasi-public uses and the above specified uses shall not apply to any lawful uses existing or approved prior to May 2, 2002, except that such uses shall not be allowed to expand beyond the legal parcel area they occupied on May 2, 2002, plus any parcel adjacent to the parcel occupied on May 2, 2002.



TOD and Gateway Office
Overlay Zones

8.0 TOD OVERLAY ZONES

Development within the TOD overlay zones shall conform to the Development Standards for the underlying zoning district, unless otherwise noted in this section.

Development within the TOD overlay zones shall incorporate measures that would encourage the use of transit, walking and cycling. For this reason transit service retail is allowed in the R4-TOD district. Additionally, developments within the TOD district shall include, but not be limited to, the following:

- Adherence to the Design Guidelines of this plan which provide direct and attractive pedestrian connections between residential and commercial uses and transit stations, and the organization of buildings to reinforce the transit stations and pedestrian routes;
- Incorporation of retail support shops and services, within the R4-TOD district could include: restaurants, cafes, exercise facilities, dry-cleaners, daycare, video rental, automated teller machines, and other services that residents and employees use on a fre-

quent basis. These same uses are encouraged for the MXD-TOD as well;

- Participation in the VTA's EcoPass Project or similar programs;
- Provision of bicycle facilities and showers (new office and employment uses);
- Drive-thru windows shall not be permitted; and
- Parking reductions shall be granted as discussed in Section 4.0.

9.0 Gateway Office Overlay Zone

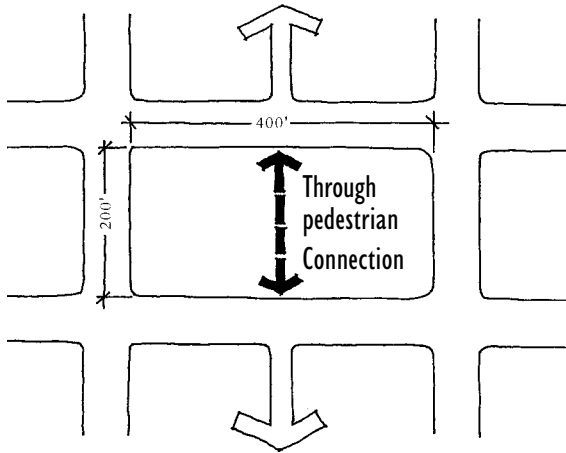
Development within the Gateway Office overlay zones shall incorporate measures to ensure an attractive, landmark-quality entry image to Milpitas, and encourage the use of alternative modes of transportation.

TABLE 8.1
Development Standards Matrix

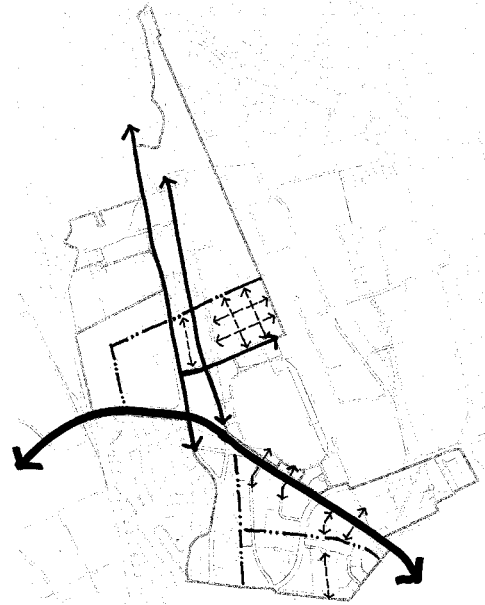
	Multifamily Very High Density (R4)	Mixed-Use (MXD)	R4-TOD	MXD-TOD	M/M2 TOD	Gateway Office (C2-OO)
Density	31 dua (min) 40 dua (max)	21 dua (min) 30 dua (max)	41 dua (min) 60 dua (max)	31 dua (min) 40 dua (max)		
FAR	N/A	0.75	N/A	1.0	0.4	1.5
Building Height	4 stories, 60 feet	3 stories, 45 feet	5 stories, 75 feet	4 stories, 60 feet	5 stories, 75 feet	6 stories, 85 feet Conditional Use – 8 stories, 115 feet max
Building Setback		<i>Vertical Mixed-Use & non-residential</i> –0 feet <i>Residential</i> –8–15 feet	Same as base	Same as base	Same as base	10 feet min
<i>Front</i>	8 – 15 feet					
<i>Rear</i>	10 feet min	10 feet min				
<i>Side</i>	10 feet min	10 feet min, 0-foot ground floor retail				
<i>Streetside</i>	8–15 feet	8–15 feet				
Open Space	3.5 acres/1,000 population	3.5 acres/1,000 population				
Street Parking		Single Family–2.0 spaces/du <i>Studio</i> –1.0 spaces/du <i>1 Bdrm</i> –1.5 spaces/du <i>2 Bdrm</i> –2.0 spaces/du <i>3 Bdrm</i> –2.0 spaces/du <i>Guest</i> –15% of required spaces <i>Bicycle</i> –5% of required spaces <i>Other Uses</i> –refer to zoning code	20% reduction of required parking	*20% reduction of required parking	20% reduction of required parking	3.3 spaces/1,000 square feet
		<i>Studio</i> –1.0 spaces/du <i>1 Bdrm</i> –1.5 spaces/du <i>2 Bdrm</i> –2.0 spaces/du <i>3 Bdrm</i> –2.0 spaces/du <i>Guest</i> –15% of required spaces <i>Bicycle</i> –5% of required spaces <i>Retail</i> –4 spaces/1,000 square feet <i>Restaurant</i> –refer to zoning code <i>Public/Quasi-Public</i> –refer to zoning code <i>Office Building <1,000 square feet</i> –5 spaces/1,000 square feet <i>Office Building >1,000 square feet</i> –3.3 spaces/1,000 square feet <i>Other Uses</i> –refer to zoning code				

* based on final location of BART station

DESIGN GUIDELINES



Block dimensions and street grid.



Street grids at Capital and Curtis Avenues, and Main and Abel Streets to the north.

DESIGN GUIDELINES

The Design Guidelines include both general design guidelines and specific standards to guide future development within the Midtown Area. The guidelines are intended to guide phased development over a 20-year period. The Design Guidelines include both mandatory standards and interpretive design guidelines. The word “should” means that an action is required unless a determination is made that the intent of the guideline is satisfied by other means. Please note that these guidelines are minimum requirements, and developers may be required to provide additional amenities to meet the goals of the Specific Plan.

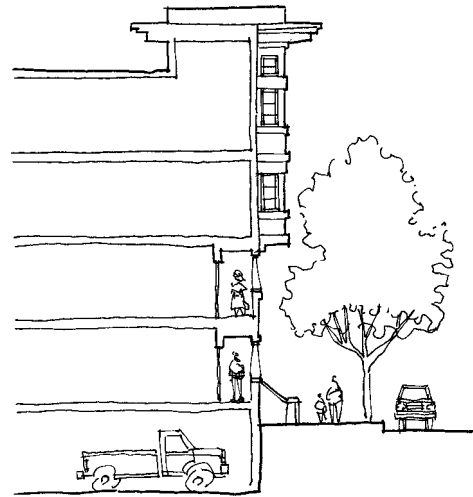
A. SITE PLANNING

I. Street Pattern

- a. The street pattern in the Multifamily Residential District should maximize connectivity through the district for both autos and pedestrians.
- b. North of the Penetencia Creek corridor, Great Mall Parkway/Capitol Avenue together represent the primary “spine” and streets should run parallel and perpendicular to it.
- c. South of the creek, the primary orientation should be north–south.
- d. In the vicinity of Curtis Avenue, streets should orient to the grid created by Curtis Avenue, Main and Abel Streets.
- e. Block lengths should not exceed 400 feet between streets and should have some form of publicly-accessible pathway at least every 200 feet. This may take the form of a pedestrian accessway or a vehicular accessway with walkways.



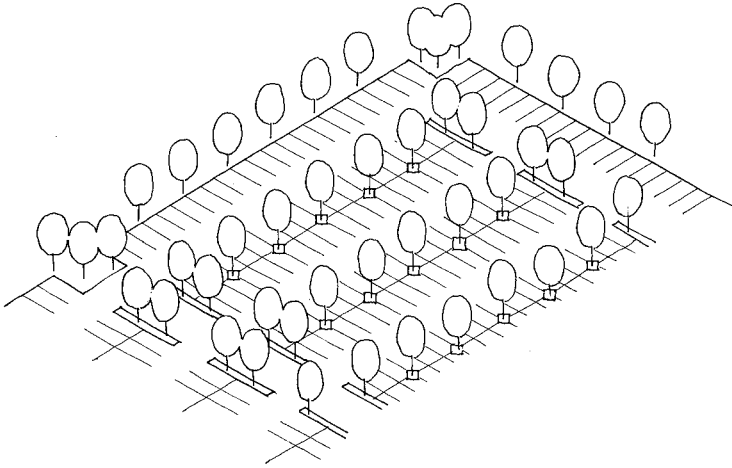
The Crossings in Mountain View. These residential units address the street and include private steps to each unit.



This illustrates a podium parking garage one-half level below grade with individual stairs to the units and trees along the street.

2. Site Configuration and Design

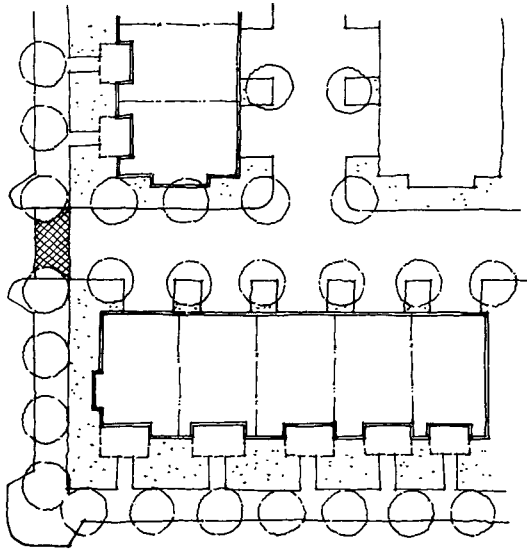
- a. Residential buildings should reinforce streets and pedestrian connections to the transit station(s) by being oriented toward the streets.
- b. Aggregation of parcels on Main and Abel Streets to create larger building sites and to reduce curbcuts is encouraged.
- c. Building facades should include street-facing entries, windows, special corner treatment, and other articulation.
- d. To mitigate the effects of adjacent service commercial or light industrial uses, increased setbacks and other measures, such as a solid 6 foot fence or masonry wall, should be considered.
- e. Primary vehicular access to all developments should be from curbcuts or accessways providing a direct connection to the street.
- f. Access drives to parking facilities should be shared wherever feasible in order to reduce curbcuts and potential conflicts with pedestrians.
- g. Street-facing surface parking lots are highly discouraged.
- h. At-grade garages for lower density residential development (i.e., rowhouses, townhouses) should be organized in well-landscaped auto mews and parking courts leading to individual garages.
- i. Security gates are prohibited in all areas of Midtown.



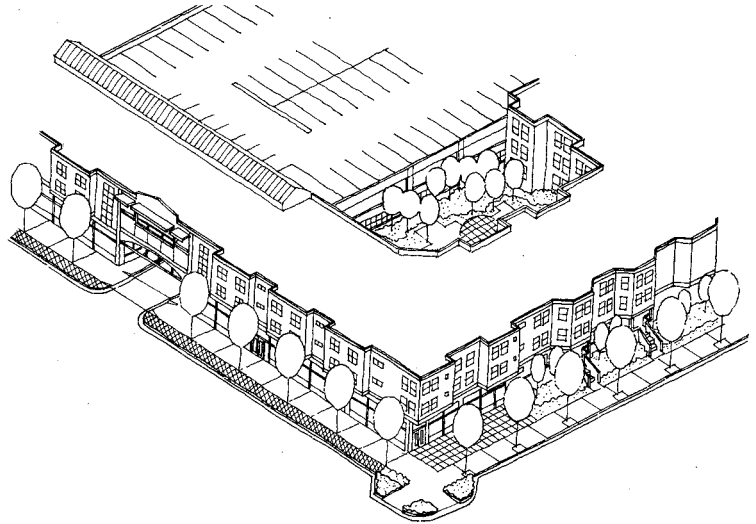
Trees planted at a ratio of 1:3 parking spaces on the perimeter of surface lots and 1:6 on the interior stalls.

3. Parking Areas

- a. Off-street parking facilities should have limited visibility (primarily entrances) from streets and accessways.
- b. Parking areas should be convenient yet not detract from the pedestrian nature of the area.
- c. Parking should generally be below grade or encapsulated within buildings to reduce the visual impact. Where not feasible, surface parking lots should be located primarily behind buildings.
- d. Surface parking areas should be well landscaped with trees planted in a regular configuration.
- e. In surface parking lots, trees should be installed at a ratio of one tree per three parking stalls for the perimeter of the parking lot, and one tree per six spaces for the interior of the parking lot.
- f. Where parking layout exceeds two rows in depth, parking should be aligned in the direction of pedestrian movement, and pedestrian island walkways should be provided within the planted area.
- g. All landscape areas should be protected with planter curbs a minimum of 6 inches-high.
- h. All perimeter setback areas should be landscaped. A screening shrub hedge (up to 6 feet high) should be planted along the property line between parcels.
- i. Parking areas within the Mixed-Use Districts should be designed in such a way to provide for a comfortable pedestrian experience.
- j. Broadleaf, deciduous trees should be used in parking lots to provide adequate shade in summer but allow sunlight to penetrate through in winter.
- k. Trees should be set into a tree grate or, landscaped median that is a minimum of 4 feet-wide (internal dimension) and well protected by tree guards or other mechanisms.
- l. The use of permeable paving or alternative materials to reduce surface runoff is strongly encouraged as a surface material for parking stalls.
- m. Within each residential units/cluster of units, an adequate amount of bicycle parking stalls should be provided.
- n. Bicycle parking should generally be secured and weather protected.



Alley-loaded parking garages accessed from the rear of the building with adequate landscaping between units.



This illustrates a parking garage entry/frontage that does not detract from pedestrian comfort and is wrapped within residential building.

4. Treatment of Garage Frontage in Residential and Mixed-Use Projects

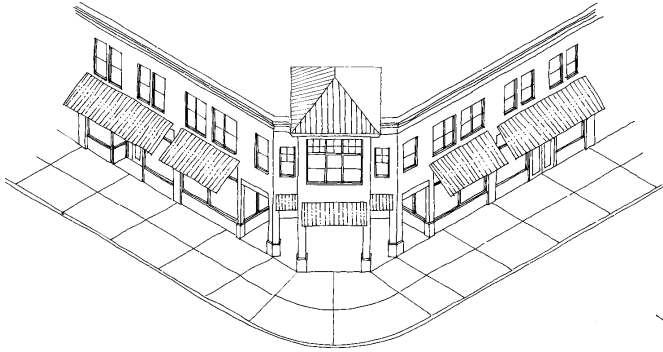
- a. For multifamily projects, service alleys or auto courts should incorporate design features to improve the appearance of the alley or auto-court.
- b. Landscaping between garages, such as vines on trellises, potted plants, or shrubs or small trees should be planted between every unit.
- c. The parking podium for multifamily buildings should be a maximum of 5 feet above grade and should be screened with stoops, stairs, ornamental screens and landscaping.
- d. Vehicular entries to garages should be from the sides or rear of buildings and not from the primary street frontage to camouflage the garage from the streets, reduce pedestrian and vehicle conflicts, and present a more attractive primary street frontage.
- e. For projects which include multiple podium buildings, shared driveways should be provided when feasible.
- f. Parking garages accessed from the front of buildings is discouraged. However, if parking is accommodated in individual unit garages that

are accessed in the front of the building, the presence of the garage should be minimized by setting the garage back at least 4 feet behind the building entry.

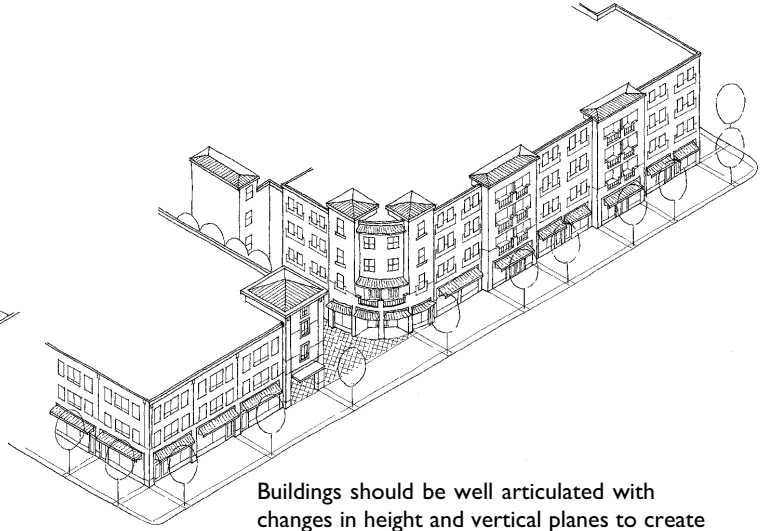
- g. For mews residential or live/work residential units, parking may be accommodated within small surface parking areas (i.e., no more than 20 spaces) or in “tuck under” garages beneath buildings.

5. Service Areas in Non-Residential Projects

- a. All loading areas should be located at the rear or sides of buildings.
- b. Loading areas should generally not be more than 30 feet from the building’s primary service entrance. They should not occupy more than 20 feet of the buildings’ rear facades.
- c. For commercial buildings, where there is no alternative, loading may occur through the front door.



Buildings should be oriented to the street with a strong entry element.



Buildings should be well articulated with changes in height and vertical planes to create an attractive streetwall.

B. BUILDING DESIGN

I. Building Orientation

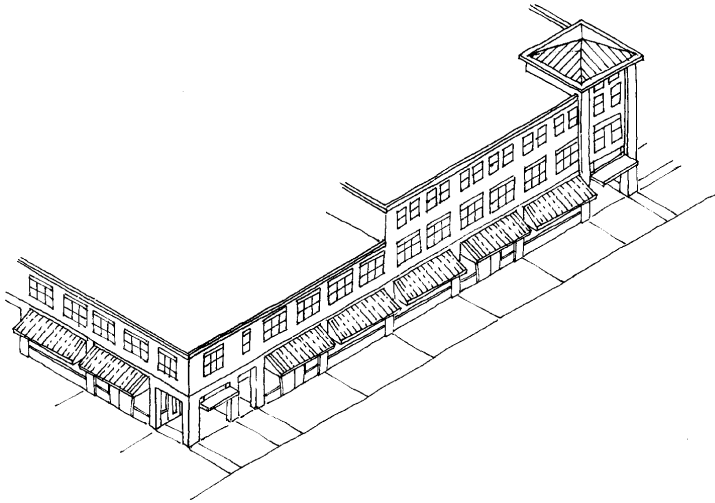
- a. Buildings should maintain a strong relationship to the street with primary building entrances oriented toward the street. Building mass should be parallel or on axis with adjacent street(s).
- b. Mews residential or live/work buildings should be oriented toward the street with windows, balconies, terraces oriented to the primary street as well as internal mews.

2 Building Massing and Articulation

- a. All exterior walls of a building should be articulated with a consistent style and materials.
- b. In no case should any facade consist of a blank wall.
- c. Buildings should be well articulated by changes in roof heights and vertical planes to reduce the appearance of bulk and create interesting building silhouettes.
- d. All building facades should have a well-defined base consisting of, but not limited to: thicker walls; richly textured materials (i.e. tile or ma-

sonry treatments); and a recognizable “top” consisting of, but not be limited to: cornice treatments; roof overhangs with brackets; stepped parapets; richly textured materials (i.e. tile or masonry treatments); and/or differently-colored materials.

- e. Building entries should be emphasized with special architectural and landscape treatment. In order to create visual interest on the other sides of buildings, secondary entrances should be treated in a similar manner.
- f. Balconies may be integrated with porches or entry features.
- g. Where units and houses face the public street, the use of balconies is encouraged for multifamily units.
- h. Upper story setbacks are encouraged especially for multifamily buildings. These setbacks should be a minimum of 6 feet or more.



Mixed-use buildings should maintain a consistent rhythm of storefronts and window rhythm with the appropriate wall-to-window ratio.



This illustrates an appropriate rhythm of storefronts on the ground floor with recessed windows and window frames above.

3. Fenestrations

- a. In mixed-use buildings, the windows should be designed to reflect the uses within, such as storefront windows at the street level and smaller windows for residential areas.
- b. Window and window frames should be set in the wall to provide a reveal (i.e., they should not be flush with the exterior face of wall).
- c. Windows should be vinyl clad, or high-quality vinyl.
- d. Window frames with high-quality metallic finishes may be allowed if found consistent with the proposed architectural vocabulary.
- e. Multi-paned windows are strongly encouraged in residential and mixed-use buildings.
- f. Snap-in plastic mullions are prohibited on street-facing facades and strongly discouraged on other facades.
- g. Window glazing should be clear or "Special E;" reflective or tinted glazing is prohibited.
- h. In multifamily and mixed-use projects, the windows visible from a street or courtyard, including those on all the facades of the buildings that front onto public or private streets or accessways, should have appropriately articulated header, jamb, and sill details to match the aesthetic of the building.
- i. In general, all windows in a residential building should have a height greater than or equal to their width, preferably with classical proportions (e.g., 2:1, 3:2, and 4:3).
- j. In residential units with narrow side yards, side elevation windows should be placed offset from those of the adjacent unit, or use obscure glass as appropriate in order to ensure privacy.
- k. Bars and security grills on windows and doors are prohibited.
- l. Doorways should be clearly identified with change in material, change in plane, or with architectural elements such as a canopy, where appropriate.



4. Building Materials

- a. All materials used should be of high-quality and properly installed.
- b. Materials selected should convey a sense of durability and permanence over any sort of architectural theme.
- c. Woodboard siding, wood shingles, tile, stucco, and/or masonry should be used. Scored plywood, such as T-111, vinyl, and aluminum siding are not allowed. If other simulated materials are used, they should be of a quality, color, and application that demonstrate a convincing realism.
- d. The primary exterior finish, whether wood or stucco, should be used on all facades of a unit or building, false-fronts are not allowed (i.e., if the front facade is primarily wood, the other facades should be wood, not stucco).
- e. Material changes should not occur at external corners, but may occur at interior corners as a return at least 6 feet from the external corners or other logical terminations.

- f. Roof materials should complement the materials and colors of the facades and provide texture or relief.
- g. Glass curtain walls and other highly reflective building materials are considered inappropriate for building walls.

5. Building Colors

- a. The body of the building or field colors should generally be more muted and light in tone. Accents, window frames, details of cornice lines etc., should be richer tones.
- b. Roofs should be mid- to dark-toned in color and complement the color of the building facade.
- c. Bright primary colors and pastels are not appropriate.
- d. Where rain gutters and down-spouts are not integrated into the exterior walls, their color should blend with adjacent surfaces.

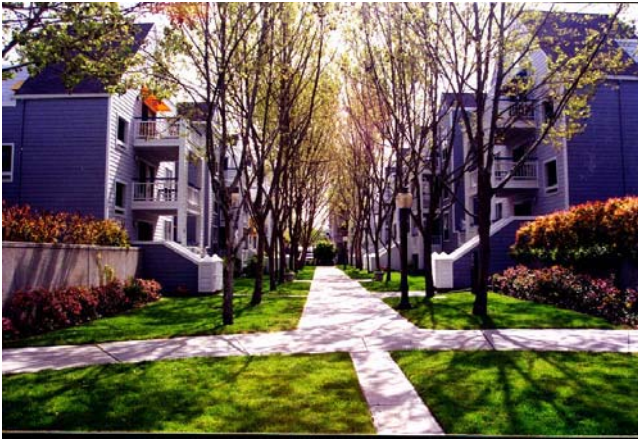
C. LANDSCAPING, SIGNAGE, & LIGHTING

6. Roof Design

- a. Roofs that have a relatively shallow pitch and deep eaves are encouraged.
- b. Buildings should have either hipped or gabled roofs with a slope no less than 4:12 and no greater than 8:12 or flat roofs with an articulated parapet.
- c. Mansard-style roofs and roofs with slopes steeper than 8:12 are discouraged.
- d. Eaves (both roof and porch) should generally be no less than 18 inches-deep.

I. Landscaping

- a. The developers of townhouses should provide base landscaping within the front and side setbacks. This would include areas of turf/native grasses, shrubbery, at least one tree per housing unit for widths up to 30 feet, two trees for widths up to 50 feet, a walkway consisting of unit pavers, and a water efficient irrigation system.
- b. The developers of multifamily and mixed-use buildings should provide full landscaping.



A highly-landscaped pedestrian walkway in a residential development.

2 Accessway and Drive Aisle Landscaping

- a. Pedestrian walkways should be heavily landscaped providing a buffer between the path and the adjacent residential units.
- b. Unit pavers provide an attractive level of detail, and should be used at key gathering areas or intersections of paths.
- c. Vehicular accessways should be landscaped similar to adjacent streets, with tree spacing typically 20 to 30 feet on center (depending on the species used).
- d. Where the side yard space between residential buildings abuts a drive aisle, a 6-foot-wide planter should be provided with a street tree and ground cover.
- e. At the end of a residential drive aisles, a 6-foot-wide planting bed should be installed and be significantly planted to provide a green terminus.

3. General Planting Guidelines

- a. The guidelines below will help ensure a healthy, attractive, and sustainable residential landscape.
 - Native and drought-tolerant plant materials are strongly encouraged. Where recycled water is or will be available, use plant species tolerant of the water source.
 - Mulched planting beds are encouraged to be utilized as a replacement for turf areas. Mulches cover and cool soil, minimize evaporation, reduce weed growth and slow erosion. Acceptable organic mulches include bark chips, wood grinding (from non-infected wood sources), or leaves. Sheet plastic in planting areas should not be used.
 - For efficient water use, irrigate turf areas separately from other plantings. Landscape plantings should be grouped according to similar water needs.
 - Trees, shrubs, flowers and ground covers can be watered efficiently by an automatic system with low volume drip, spray, or bubbler emitters.



Projecting signs from storefronts.

4. Signage

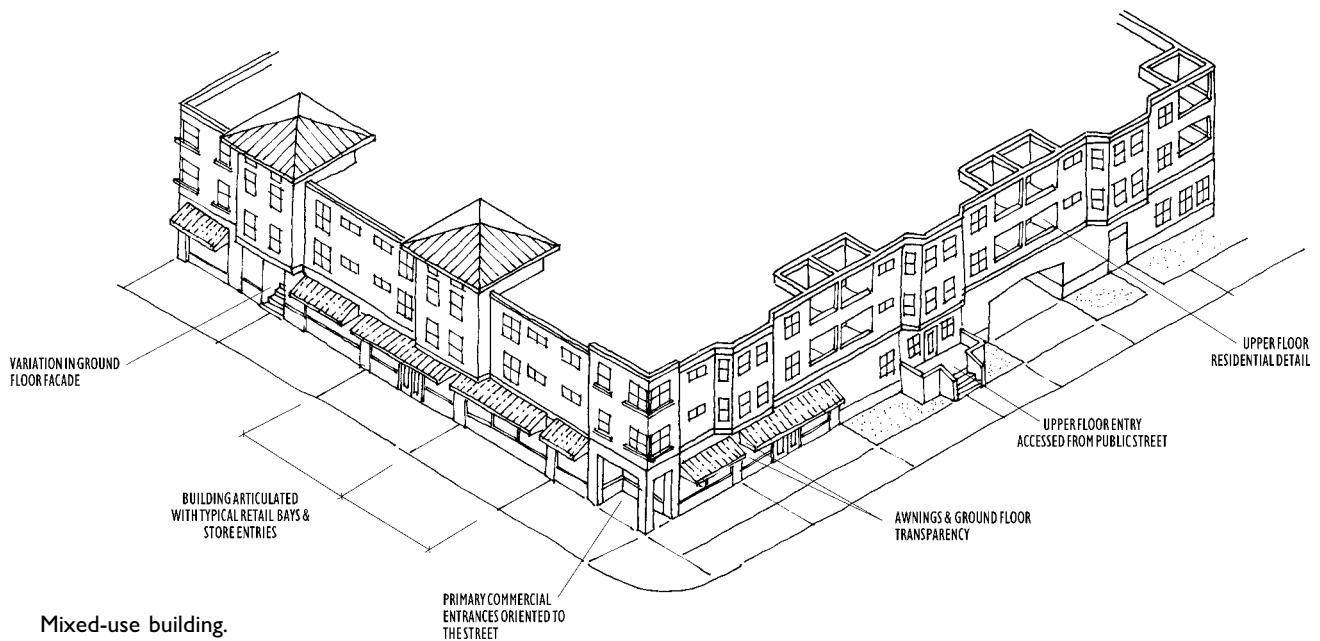
- a. A coordinated signage plan should be included for all multi-tenant buildings.
- b. Freestanding signs are discouraged, except at a single major site entry.
- c. Animated, moving, flashing, blinking, reflecting and revolving signs are prohibited.
- d. Cabinet signs are prohibited.
- e. All signs should be designed to complement the architectural style and setting of the structure or use it is adjacent to. Building wall and fascia signs should be compatible with the predominant visual elements of the building.
- f. Signs should be an integral part of the design of the storefronts in mixed-use buildings.
- g. The size of signs and sign letters should be proportional to the space they are located in, with the letters typically between 6 and 16-inches high.
- h. Sign letters and materials should be professionally designed and fabricated.
- i. Primary signs should contain only the name of the business and/or its logo.
- j. Signs should be constructed using high-quality materials such as metal, stone, wood.
- k. Exposed conduit and tubing is prohibited. All transformers and other equipment should be concealed.
- l. Projecting signs mounted perpendicular to the facade of the building should be located at least 8 feet above the sidewalk. The outside edge should be no more than 5 feet from the face of the building.
- m. Window signs should not exceed 15% of the window area. Signs should not obstruct visibility into and out of the window.
- n. Window signs may include one “open” or “closed” sign less than 2 square-feet.
- o. While bilingual signs are allowed, the size of English lettering should be at least equal to the size of letters of another language.



This illustrates a pedestrian-scale light fixture (between 12–16 feet) along a storefront sidewalk.

5. Lighting

- a. Lights should be designed and placed to direct lighting to appropriate surfaces and minimize glare into adjacent areas.
- b. The light source used in outdoor lighting should provide a white light for better color representation and to create a more pedestrian-friendly environment.
- c. Low pressure sodium lamps are prohibited.
- d. To reinforce the pedestrian character of the area, light standards along sidewalks should not exceed 12 to 16 feet in height.
- e. The use of uplighting to accent interesting architectural features or landscaping is encouraged.



D. DESIGN GUIDELINES BY BUILDING TYPE

I. Mixed-Use Buildings

- a. Mixed-use buildings, which contain a vertical combination of residential and commercial uses within a single building, are encouraged in the Midtown Area.
- b. The mix of uses in vertical mixed-use structures should be carefully chosen and located for maximum compatibility and mutual benefit, as follows:
 - Retail uses should be generally limited to the ground-floor spaces along the street and prominent pedestrian promenade frontages;
 - Commercial uses within mixed-use projects should best serve the surrounding neighborhood and/or promote pedestrian traffic or public transit. Such uses may include, but are not limited to; childcare centers, cafes, dry cleaners, automated teller machines, video rentals, small groceries, newsstands, etc.; and
 - Commercial hours of operation should not conflict with adjacent residential uses.
- c. The primary facades of all buildings in the Mixed-Use District should face the street.
- d. Mixed-use buildings should have a building form that blends with the residential buildings that surround them.
- e. The ground-level should achieve maximum transparency, avoiding areas of blank walls.
- f. Ground-floor commercial uses should have an architectural design similar to traditional street front businesses, with large storefront windows, and easily accessible, clearly defined entries.
- g. The ground-floor area facing the street should be designed for retail use with taller floor to ceiling heights with a minimum height of 14 feet.
- h. Mixed-use buildings should be developed with a rhythm in keeping with the desired pedestrian scale and character. Commercial (retail and office) bays should be between 20 and 40 feet.
- i. Variations in floor level, facades such as shallow recesses at entries, or arcades are encouraged, for they create the appearance of several smaller buildings and shops, rather than a single, large and monotonous building.



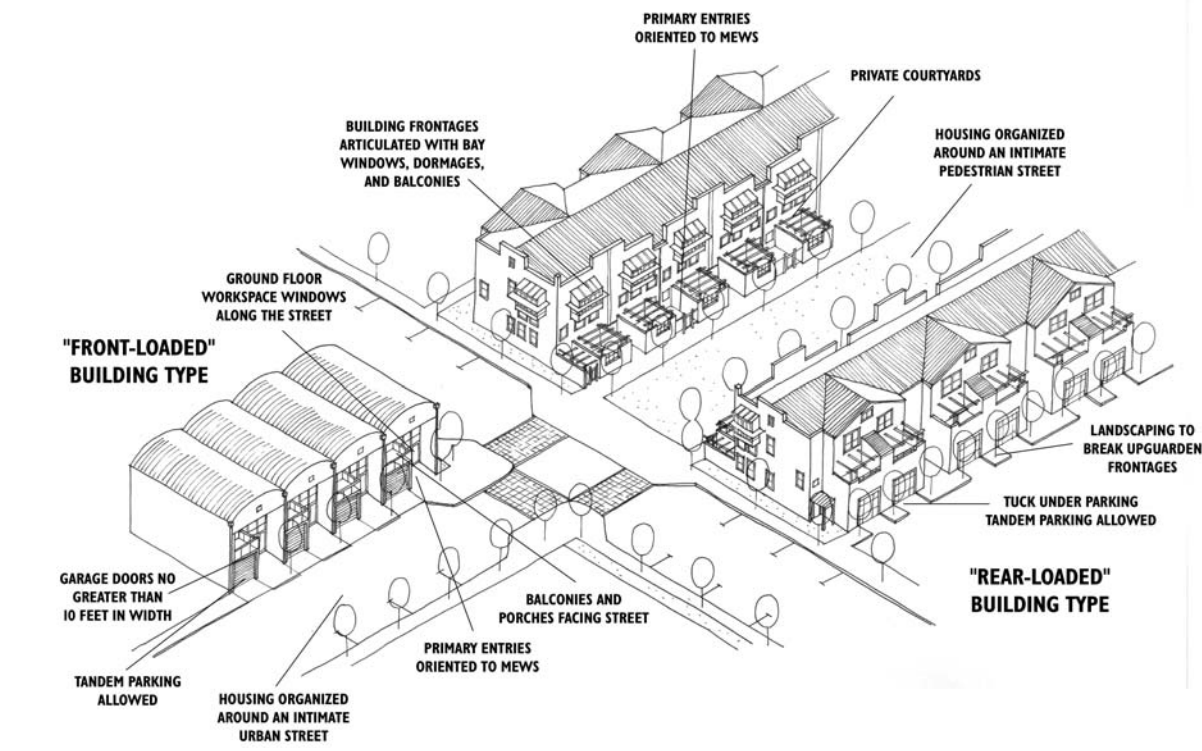
Lorin Street, Berkeley, CA.



Orenco Station, Hillsboro, OR.

These mixed-use developments include a range of housing units, office and retail space. They both have ground-floor retail with primary entrances oriented toward the street.

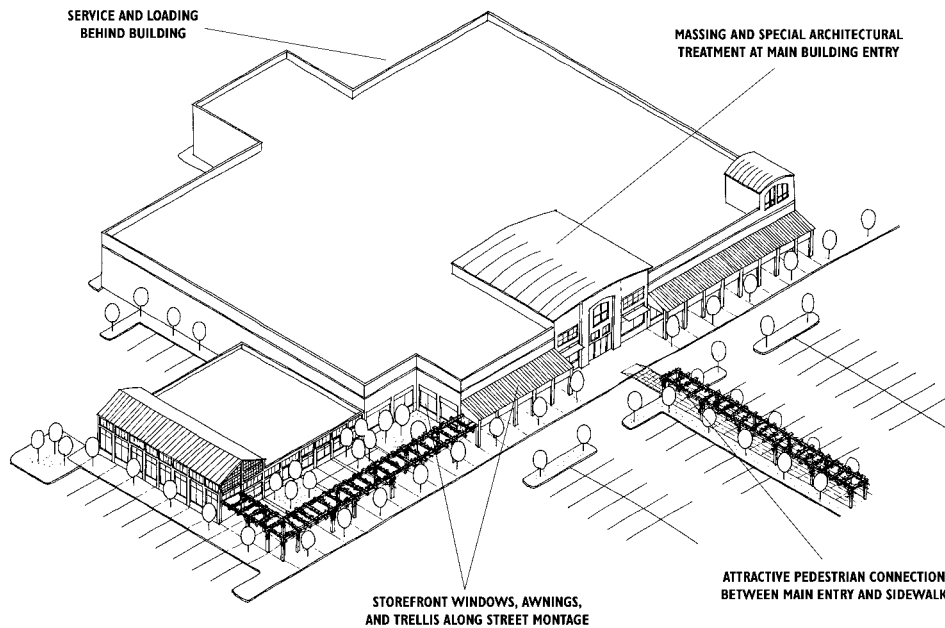
- j. Primary facades should be built parallel to the street.
- k. All commercial uses should have their primary entrances oriented toward the street, and entrances should be spaced no more than 50 feet apart.
- l. Blank walls should not occupy over 30% of the principal frontage, and a section of blank wall should not exceed 20 linear feet without being interrupted by a window or entry.
- m. Windows should encompass a minimum of 50% of a building's primary facade and a minimum of 30% of other building facades in order to create visual interest on all sides of the building.
- n. Ground-floor elevations should vary no more than 2 feet from sidewalk level.
- p. The primary entry(s) for commercial establishments and the entrances to the second floor residential units should be within the primary facade, and should be visible and accessible directly from a public street.
- q. In order to create visual interest on the other sides of buildings, secondary entrances should be treated in a similar manner as the main entry (although to a suitably lesser degree).
- r. The use of awnings is encouraged to provide shelter and shade along the sidewalk. Awnings should be no wider than a single storefront or architectural bay (whichever is narrower).
- s. Upper floors should have smaller window openings punched into solid walls.
- t. Upper floor residential uses should be detailed with porches, bay windows, dormer windows, and/or balconies.
- u. Curtain walls are prohibited.



2. Multifamily Residential

- a. Multifamily buildings should be well articulated to break up the building mass. Variations in floor level, facades, roof styles, architectural details, and finishes that break up the appearance of large buildings should be employed.
- b. Street-facing facades of residential buildings should include stoops, porches, recessed windows, bay windows, and balconies in order to provide visual interest.
- c. Ground-floor units of multifamily residential units facing the street should be accessed directly from the street.
- d. The first floor should be no more than 5 feet above the sidewalk elevation.
- f. Porches, bays and balconies are required along street facades and may extend into the setback areas. Porches are required along at least 30% of the ground level of each unit.





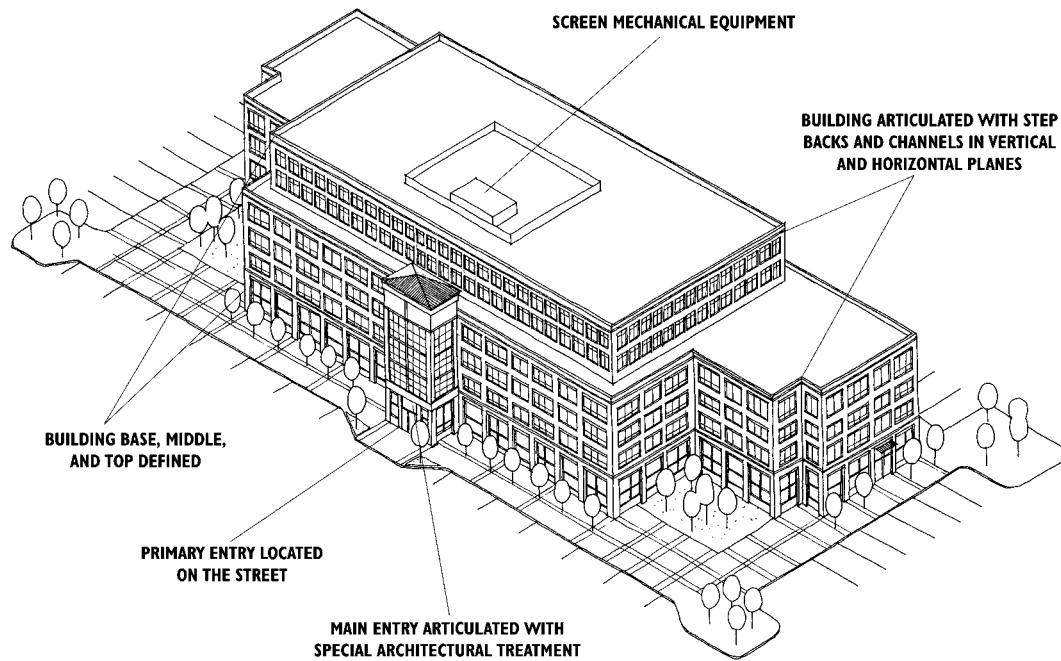
Big-Box Retail Building

3. Large Floorplate or Big-Box Retail

- a. Although big-box retail uses are primarily automobile-oriented, they should be designed to accommodate pedestrian and bicycle traffic as well, given the nearby locations of transit and higher density residential development.
- b. Building entries should be articulated with taller elements and with elements such as canopies.
- c. Buildings located at gateway intersections should include corner vertical elements to emphasize entries.
- d. Entries may orient to parking areas, but continuous sidewalks should be provided from the primary street directly to the doorway.
- e. A continuous arcade is strongly encouraged along the front facade.
- f. Building facades should be articulated with a combination of windows, entries and bays.
- g. Street-facing blank walls are strongly discouraged. Where they cannot be avoided, a permanent trellis should be planted with vines or other architectural and landscape design elements should be incorporated into the building design to reduce the visual impact of the blank wall.
- h. A small plaza is encouraged at the building entry to visually define the feature.



A trellis through the surface parking lot to enhance pedestrian connection to main store entrances.



Class A Office Building

4. Office Buildings

- Street- and plaza-facing facades should be lined with windows.
- Blank walls should not occupy over 30% of the principal frontage, and a section of blank wall should not exceed 20 linear feet without being interrupted by a window or entry.
- Vertical building elements should be used to break up what may otherwise be a horizontal architectural composition.
- Elements such as awnings, arcades, porches, or porticos should be incorporated along the street-facing facades.

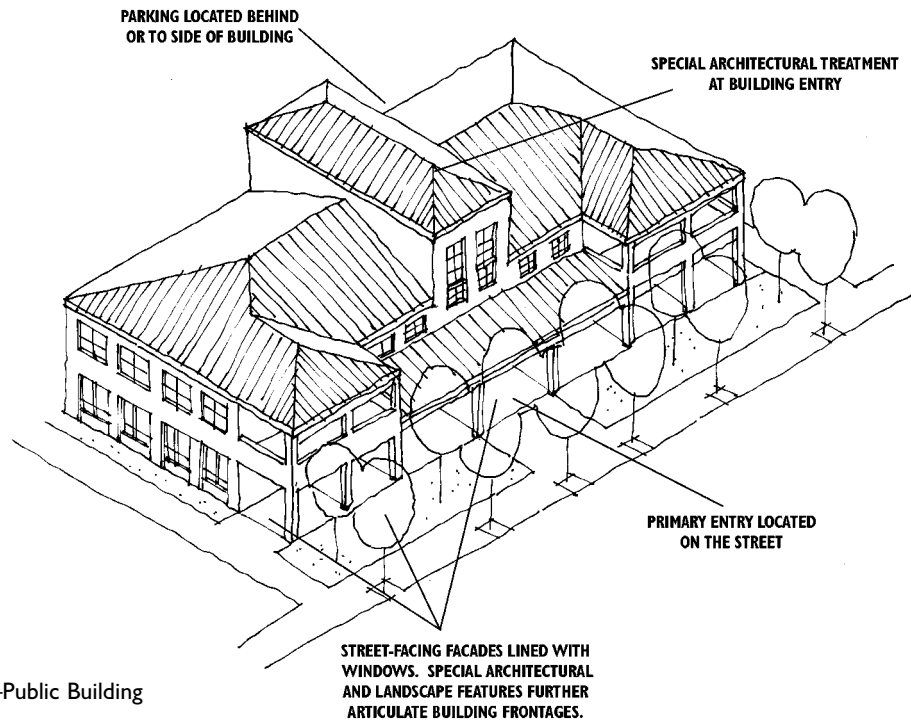


An office building that faces the street, has a distinguishable entry, provides service commercial uses on the ground-floor and has adequate floor-height proportions.

Class A Office Buildings

Class A office buildings are defined as high-quality office buildings with amenities that typically attract rents in the top 25% bracket.

- The base of the building facing the street should be designed to include retail uses (or service commercial uses).
- The floor to ceiling height of the first floor should be greater than the floor to ceiling heights of the upper floors and should generally be between 14 and 16 feet.
- The building form should incorporate a distinguishable base, a middle and a top.
- The architectural materials and designs should be of high-quality.
- The building base should be articulated either with a change in materials, color and finishes, fenestration pattern and size, and an emphasized building entrance or arcade.
- Quality materials that are durable and provide a sense of permanence should be used throughout the building.



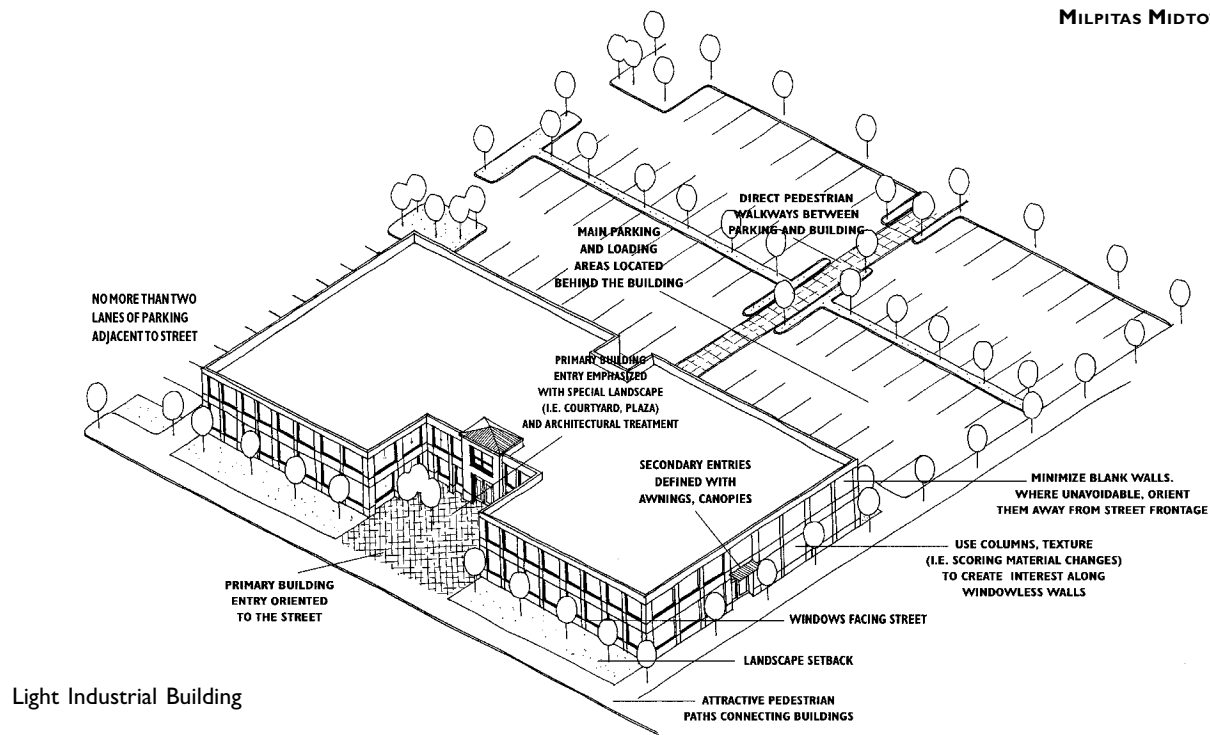
Civic, Public and Quasi-Public Building

6. Civic, Public and Quasi-Public Buildings

- g. Additional accent materials such as tile insets or natural stone should be used at the base of the building to provide added texture, color and visual interest at the pedestrian level.
 - h. Building entries should be clearly defined and designed to be clearly identifiable from the street.
 - i. Rain gutters, scuppers and other drainage devices should be incorporated into the structure of the building.
 - j. Development within the Gateway Office overlay zones must incorporate measures to ensure an attractive, landmark quality entry image to Milpitas, and encourage the use of alternative modes of transportation.
- a. Primary building entries should be oriented toward the street, with attractive pedestrian walkways to the sidewalk.
 - b. Street- and plaza-facing facades should be lined with windows.
 - c. Public buildings should have a prominent building entrance defined by architectural and landscape features, such as tower elements, canopies, columns, recesses, plazas and landscaped open space.

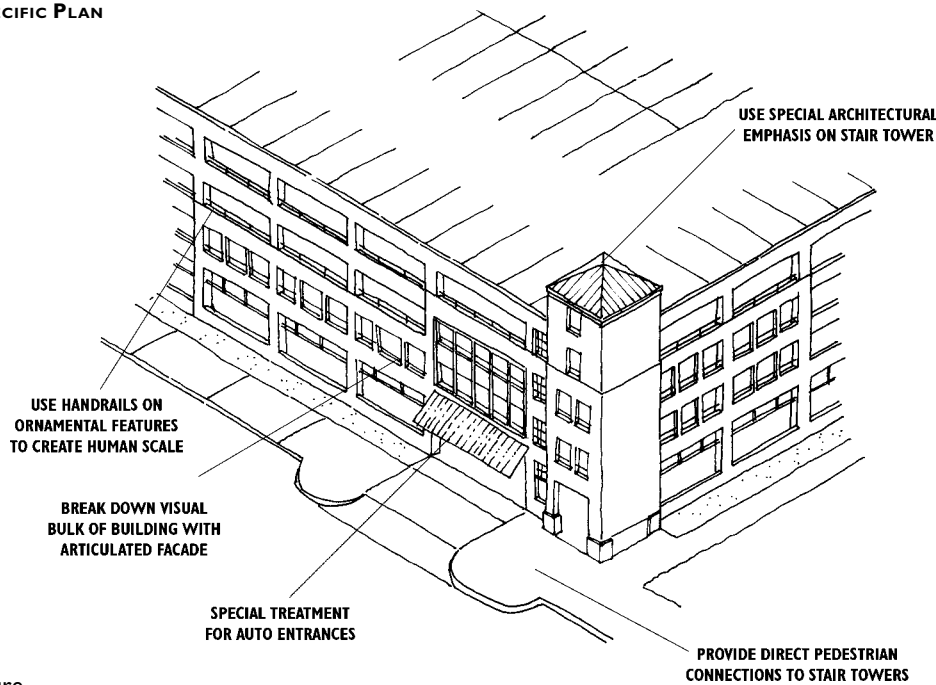


Berkeley Library, Berkeley, CA.



7. Light Industrial/Industrial Park

- New office/industrial buildings should be oriented toward the street, with parking areas located to the side and behind buildings.
- A direct pedestrian connection between the street and sidewalk and the building entry or entry plaza is required.
- The primary building entry should face the street and should be clearly defined with special massing and landscape treatment to make it stand apart from the rest of the building.
- Buildings should be comprised of bold simple forms with highly articulated exterior planes and openings to provide an interplay of shadow and light and create a visual interest.
- The building should be sited and designed to reinforce the street edge or corner, where appropriate.
- The building mass should be broken up with arcades, balconies, and terraces to avoid a monotonous appearance.
- The use of architectural features, such as porticos, canopies, or arcades, special roof treatment and/or landscape treatment, such as entry plazas or courtyards should be used to create an easily identifiable entry.
- The use of industrial materials and accent features is encouraged to animate building facades and entries. These features may include: window canopies; cornice projections; tension cables to support entry canopies or trellises; structural pilasters or columns; fin walls which project from entries of window groupings; window mullions; and/or mechanical screens.
- When located to the side of buildings, parking should generally not consume more than 30% of the street frontage.



Parking Structure

8. Parking Structures

- a. To the extent feasible, parking structures should be located away from prominent pedestrian streets.
- b. Parking structures should be designed in keeping with the character of the primary buildings on or near the site.
- c. Parking structure facades should be designed as compatible visual extensions of other multistory buildings.
- d. If feasible, active ground-level commercial uses should be incorporated into parking structures along the sidewalk.
- e. Auto entries should be located in a manner that minimizes pedestrian/auto conflicts.
- f. Openings should be carefully composed within the building wall to appear as well proportioned windows rather than continuous open strips.
- g. Variation in the dimension and proportion of openings and in the horizontal and vertical planes of the facade should be provided to create visual interest and to reduce the mass of the parking structure.
- h. Decorative screen and trellis elements of durable, high-quality materials are encouraged to provide variation and interest on the facade.
- i. Building detailing such as ornamental metal hand railings should be used to create human scale and interest.
- j. Entries and stairwells within parking structures should be located adjacent to public street and designed to be visually open, to promote a feeling of security and comfort.
- k. Stair towers should be designed as identity elements.

E. DESIGN GUIDELINES FOR SPECIFIC PROJECTS

The Specific Plan envisions several areas within the Midtown Area as being “special” places such as the proposed Town Square and the southern multi-modal transit station. Another area that is envisioned to have a unique development pattern within the Midtown Area is the residential development along South Main Street. The guidelines below address each of the three areas.



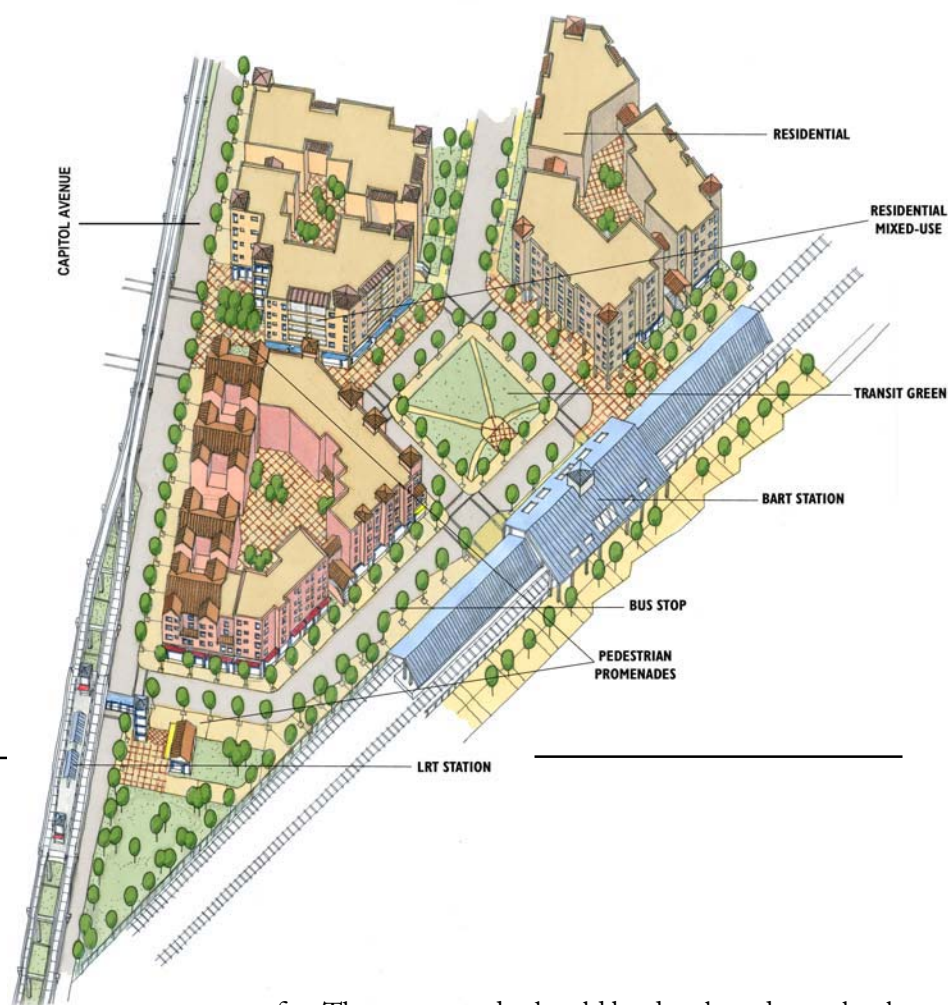
Illustrative Concept: Serra Way and Main Street with Town Square.



I. Town Square

- a. The minimum size for the Town Square should be 8,000 square-feet with the minimum frontage on Main Street 75 feet.
- b. The Town Square should provide opportunities for public gathering. This would include landscape features such as:
 - A turf grass lawn;
 - A hard-surfaced area;
 - Raised landscaped features (planters) designed for informal seating;
 - A seating area shaded by a grouping of flowering trees; and
 - Moveable and permanent seating and tables both public and/or cafe/restaurant.
- c. The Town Square should have its primary frontage onto Main Street and be publicly-accessible throughout the day.
- d. Surrounding buildings should have entries, windows, and seating areas facing the town square.
- e. Sidewalks and planting strips along the street are considered to be part of the square perimeter and both the character of the square and the established identity of the street as a whole should inform their design (i.e. tree and plant selection, paving, dimensions, etc.).
- f. The square should have a civic quality of landscape design and not appear as part of a development project.
- g. The Town Square should be designed to provide the user with a year-round choice as to seating preferences by providing a balance of shade and sun-exposed areas. Some areas may be protected by gazebos or other overhead structures to protect users from seasonal rains.

Illustrative Concept: South
Midtown Transit Promenade



2 Southern Transit Station

- a. The proposed Capitol/Montague LRT Station should be conveniently accessible and designed to be an attractive neighborhood element.
- b. Strong pedestrian connections should be developed between the station and adjacent neighborhoods, the trail system, parking structures and surface parking lots.
- c. The pedestrian connections should be developed with broad (i.e., minimum 15-foot-wide) treelined sidewalks with pedestrian-scaled (i.e., 12 to 16 feet-high) light fixtures.
- d. The majority of parking for the proposed BART facility should be located east of the station, while a residential transit village should be located west of the station.
- e. The transit promenade refers to the future development that will be organized around the Capitol LRT Station and future BART station.
- f. The promenade should be developed as a clearly recognizable and attractive transit node and small gathering area.
- g. The promenade should have convenient and attractive pedestrian linkages between neighborhoods and the transit stops and should include a small green or plaza.
- h. Where feasible, the promenade should be bounded by mixed-use buildings offering goods and services to commuters and residents such as a cafe, dry cleaners, video store, banks, child daycare or automated teller machine in order to establish the transit area as a neighborhood activity center.
- i. Where the promenade passes through a mixed-use area, it should offer easy access to shops and the transit green.
- j. Where it passes residential buildings or rail tracks, buffer planting should be installed. Buffer planting near residences will offer privacy, while buffer planting near rail lines will create a more attractive walking environment.



A multifamily residential development with below grade parking and street orientation.



A high-density residential development that has a landscaped setback and building articulation.

3. South Main Residential Development

- a. Existing properties along South Main Street (south of Great Mall Parkway) are encouraged to be aggregated to support the desired development patterns as described in the Specific Plan.
- b. In this area, it is not possible to develop typical street and block patterns, due to the configuration of land adjacent to the railroad corridor. These areas should be arranged in a mews configuration, which provides an internal focus, as well as a street orientation.
- c. Mews should be well-landscaped and display the character of a small urban street. Where feasible, planter beds with trees or potted plants should be located between garage doors and adjacent to porches.
- d. Off-street parking should to be provided within individual garages and along the mews.
- e. A visual connection between the mews and South Main Street should be at the access points from the street.